



Report of the Chief Planning Officer

CITY PLANS PANEL

Date: 11th April 2024

Subject: Planning Application 23/06266/FU for the construction of an office building with associated facilities, parking and landscaping at a site to the north of Whitehall Road (land at the former Doncaster Monk Bridge Works) Leeds.

APPLICANT

BAM Monk Bridge Limited

DATE VALID

16 October 2023

TARGET DATE

15 January 2023

Electoral Wards Affected:

Hunslet & Riverside

Specific Implications For:

Equality and Diversity

Community Cohesion

Narrowing the Gap

RECOMMENDATION:

For Members to note the contents of this report which is provided by way of an update to the report to 15th February 2024 City Plans Panel and to Defer and Delegate to the Chief Planning Officer for approval, subject to the conditions at Appendix 2 (and any amendment to or addition of others which the Chief Planning Officer considers appropriate), and subject to resolving technical details regarding highways matters and Active Travel England comments and also subject to the completion of a Section 106 agreement to secure the following:

- 1.0.91 Biodiversity Units to be Habitat Units delivered off site in the same locality – £25,000
2. On site Public Realm areas are publicly accessible
3. Travel Plan review fee – £5504
4. Contribution for free trial membership and usage of the car club by occupiers of the development and/or other sustainable travel measures for the employees e.g. public transport tickets, hire bikes – £22,000
5. Globe Road/Whitehall Road Junction improvements – £188,250
6. Management Plan to cover maintenance of Whitehall
7. Wayfinding – £16,000
8. Employment and training of local people

9. Monitoring fees.

In the circumstances where the Section 106 has not been completed within 3 months of the Panel resolution to grant planning permission, the final determination of the applications shall be delegated to the Chief Planning Officer

Conditions

A list of draft Conditions for the application is provided in Appendix 2 of this report.

1.0 INTRODUCTION:

1.1 The proposal was originally put before City Plans Panel, in accordance with Exception 1(g) of the Officer Delegation Scheme “*where the Chair in conjunction with the Chief Planning Officer considers that the application should be referred to the relevant Plans Panel for determination because of the significance, impact or sensitivity of the proposal*”. It was deferred at City Plans Panel on 15th February 2024. The previous report is attached as an Appendix of this report. The previous report contains the appropriate policy background and substantive analysis of the application as previously submitted and the proposed draft planning conditions and obligations. Members are reminded that this previous substantive report is also to be considered in addition to this update report.

1.2 At the 15th February City Plans Panel members expressed concerns regarding the western landscaped areas and the amount of natural surveillance which would be offered to this space. Members resolved that the application be deferred to allow for further information to be provided with regards to security issues regarding the screened footpath and the permeability of the screens. This matter is addressed at section 4.0 of this update report.

2.0 SITE AND SURROUNDINGS, RELEVANT PLANNING HISTORY AND RELEVANT PLANNING POLICIES:

2.1 These matters are addressed – in sections 3.0, 4.0 and 7.0 of the Appendix 1

3.0 PUBLIC/LOCAL RESPONSE:

3.1 The 15th February 2024 panel report details the public consultation which was undertaken and that no comments have been received as part of the publicity period, section 6.0 of Appendix 1. However, after the 15th February 2024 report was finalised, Cllr Carlisle has made formal representations that he wishes to fully support the proposal.

MAIN ISSUES

4.0 Western landscaped area:

4.1 As part of discussions at the 15th February Plans Panel, Members raised comments regarding the proposed western landscaped area and the amount of perceived natural surveillance this area would feature, and how this space would feel at quieter times in the day, when the office use has finished. The applicant has now re-evaluated this area via a further consultation with West Yorkshire Police (WYP) and can provide the following update.

- 4.2 The north/south route would be accessed from Whitehall Road and to the side of the proposed colonnade of which a gap of 4.1m would be retained which then reduces to 3m between the corner of the substation and railway embankment. This route would only be used for people wishing to link on to Whitehall. Pedestrians wishing to continue traveling along Whitehall Road would continue along, crossing the cycle lane and continuing their journey along Whitehall Road.
- 4.3 The details of the final landscaping would be secured via condition 5 as noted in Appendix 1 and it is envisaged that planting within this space would allow for 360 views in the form of lower level planting and raised clear stem (at least 2m from ground level) canopies in order to provide natural surveillance through this space. In addition, all external lighting would be secured via conditions 5 and 38 as well. The applicant has provided further information of this space which has detailed the views which would be provided of the space from Whitehall Road and Whitehall, which would be well lit via column lighting and soffit downlighting to the ceiling of the car park area. In addition, the area will also benefit from extensive motion sensor CCTV surveillance.
- 4.4 In terms of physical eyes on this space, this area is undergoing significant regeneration and various construction works are ongoing at present. The development to the north of the site. Latitude Purple B, is due to be completed soon which will introduce apartments, of which some of these would overlook towards this space at first floor and above. In terms of the ground floor of Latitude Purple B, this will feature an entrance point to the south west corner of the building, which will introduce activity and additional eyes on to this space, of which a direct line of sight will be provided towards the south and a retained gap of 13.4m between the colonnades and the embankment along Whitehall would also be provided.
- 4.5 In addition, it is acknowledged that due to various wind safety exceedances on site wind mitigation is required in the form of panels within the colonnades of the building to mitigate these exceedances. The final details of these panels would be secured via condition 36 but in addition, and in response to Members comments it has been confirmed that the screens within the colonnades are perforated to allow visual connections through them and afford surveillance from the main roads into the spaces beyond. Lighting is also now shown around the columns of the wind mitigation with the garden behind also benefiting from extensive lighting. Therefore, anyone looking through the panel, from Whitehall or Whitehall Road, would see through sufficiently like they could during the day. The panels are 50% perforated, which are similar to other screens which can be found in the city centre. Examples of these are Leeds Vita Building and Leeds Beckett University - School of Arts. These existing panels allow views through and therefore natural surveillance will be retained towards this proposed landscaped space, which will be further highlighted via the lighting proposed, secured via condition.
- 4.6 In regard to the ongoing management of the building, it has been confirmed by the applicant that there will be 24 hour security in the building for the life of the development and that the surveillance levels will be the same during the day and night as there will be a security guard monitoring the CCTV at these times.
- 4.7 As part of the initial application process West Yorkshire Police (WYP) were consulted and raised no objections to the proposal provided that final security details are secured via condition and implemented as part of any approval. Since deferral of the application a further consultation has taken place with WYP to set out the measures listed above, of which WYP Officers have responded that *“due to the proposed*

measures as set out by the developer, we are happy that the measures, as demonstrated, will appear to create a well-lit, monitored, and safe environment.”

4.8 Officers consider that the development of this site will also have a benefit to the people using this site/walking by/through, via the activation of the area and the proposed enhancements in the form of extensive lighting and CCTV, and a greater level of activity, when compared to the footpath which currently runs through this cleared site.

5.0 **PLANNING BALANCE AND CONCLUSION**

5.1 The applicant has responded positively to address the concerns raised by Members and have collaborated with officers and West Yorkshire Police to verify that the landscaped area has been designed to feel safe and the use of external lighting and CCTV will help to promote this feeling. In addition, the wind mitigation will be designed so there are views through this, similar to what exists within other city centre sites.

5.2 The identification of this site and wider area within the Site Allocations Plan (SAP) recognised this area of the city for development. The proposed development follows similar principles already established as part of the original Masterplan of this site, which were originally approved in 2007 and have been implemented.

5.3 The development would involve the further regeneration of a key brownfield allocated site within the city centre boundary. The site is in a highly sustainable location, 20-minute walk to Leeds train station and within easy access to numerous bus stops. The development of this site would also contribute to the mix of developments within this area of the city centre and would also introduce new employment, public landscaped areas, improved pedestrian and cycle connections and add to the vibrancy and vitality to the area and further the regeneration to this part of the city centre.

5.4 As a result, on balance it is considered the development accords with the Development Plan as a whole. Accordingly, it is recommended that the scheme should be deferred and delegated to the Chief Planning Officer for approval subject to resolving technical highways matters and Active Travel England comments, the draft conditions specified in Appendix 2 (and any amendment to the same or addition of further conditions as the Chief Planning Officer deems appropriate) and the completion of a Section 106 agreement to secure the obligations listed in paragraph 10.49 of appendix 2.

Background Papers:

23/06266/FU



Report of the Chief Planning Officer

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Electoral Wards Affected:

Hunslet & Riverside

Specific Implications For:

Equality and Diversity

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Narrowing the Gap

RECOMMENDATION: Defer and Delegate to the Chief Planning Officer for approval, subject to the conditions at Appendix 1 (and any amendment to or addition of others which the Chief Planning Officer considers appropriate), and subject to resolving technical details regarding highways matters and Active Travel England comments and also subject to the completion of a Section 106 agreement to secure the following:

- 1.0.91 Biodiversity Units to be Habitat Units delivered off site in the same locality – £25,000**
- 2. On site Public Realm areas are publicly accessible**
- 3. Travel Plan review fee – £5504**
- 4. Contribution for free trial membership and usage of the car club by occupiers of the development and/or other sustainable travel measures for the employees e.g. public transport tickets, hire bikes – £22,000**
- 5. Globe Road/Whitehall Road Junction improvements – £188,250**
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9. Monitoring fees.

In the circumstances where the Section 106 has not been completed within 3 months of the Panel resolution to grant planning permission, the final determination of the applications shall be delegated to the Chief Planning Officer

Conditions

A list of draft Conditions for the application is provided in the Appendix 1 of this report.

2.0 INTRODUCTION:

- 1.1 The proposal is put before City Plans Panel, in accordance with the Officer Delegation Scheme 1(g) as the Chair in conjunction with the Chief Planning Officer considers that the application should be referred to the relevant Plans Panel for determination because of the significance, impact or sensitivity of the proposal.
- 1.2 This site has a previous planning approval as part of a wider masterplan for the Former Doncaster Monkbridge Works (06/02880/OT) for a 10-storey office building on the site. This permission was granted with reserved matters in October 2015 (13/02017/RM), with pre commencement conditions being discharged in July 2017 (17/03305/COND). Since securing planning permission, this site was marketed on a pre-let basis, however at that time the site did not appear attractive to the markets and the site has laid dormant despite the developments which have been carried out around it.
- 1.3 The application site is now considered much more connected to Leeds City Centre and there is a significant opportunity to reposition this remaining element of the Former Doncaster Monkbridge Works site (Latitude Yellow) in the marketplace to deliver a sustainable Grade A office building.
- 1.4 The Applicant's team presented the emerging pre-application proposals to City Plans Panel on 10th August 2023. At the meeting Members were generally supportive of the proposal. Member's comments from the City Plans Panel minutes are set out below in paragraph 5.2.

2.0 PROPOSAL:

- 2.1 The proposals seek to create a 12-storey office building with a public ground floor café/co working space and an external employee roof terrace which will be landscaped.
- 2.2 A total of 10 car parking spaces (including 2 disabled spaces) are proposed within the ground floor undercroft parking area, of which Electric Vehicle Charge points will be provided to all spaces. A total of 138 long stay cycle spaces are to be provided across ground and mezzanine levels. In addition, 20 short stay cycle spaces will be provided through 10 Sheffield stands within the landscaped areas.
- 2.3 The development would also feature new landscaping, public realm and improvements to cycle and pedestrian infrastructure along Whitehall Road. The landscaping would also incorporate new tree planting to the east and south with a new public space to the west of the building, featuring a mixture of soft and hard landscaping.

2.4 The application is supported with the following documents:

- I. Scaled Drawings
- II. Design And Access Statement
- III. Planning Policy Statement
- IV. Air Quality Assessment
- V. Ecological Impact Assessment
- VI. Biodiversity Metric 4.0 Calculation Tool
- VII. Desktop Solar Glare Report
- VIII. Flood Risk Assessment
- IX. Drainage Strategy
- X. Heritage Statement
- XI. Groundwater Risk Assessment
- XII. Lighting Assessment
- XIII. Noise Impact Assessment
- XIV. Phase 1 & 2 Ground Report
- XV. Bay Materiality Studies
- XVI. Travel Plan
- XVII. Pedestrian Level Wind Microclimate Assessment
- XVIII. Sustainability Appraisal
- XIX. BREEAM Pre-Assessment
- XX. Statement Of Community Involvement

3.0 SITE AND SURROUNDINGS:

- 3.1 Historically the site was home to the Monk Bridge Iron Company and then the Monk Bridge Iron Company, which created various high quality Iron and alloy steels. The site is cleared and located to the west of the City and to the north of Whitehall Road and forms part of the wider Latitude development, which is located to the east of the elevated Leeds-Bradford railway line and west of the River Aire and Leeds-Liverpool Canal. The Latitude development consists of a range of heights from 8 storeys of Latitude Red (east of this site) to 31 storeys of the Latitude purple A (north east of this site).
- 3.2 The site totals (approx.) 0.31 hectares and falls inside of the defined city centre of Leeds. The site is enclosed by post rail fencing and currently consists of predominantly a grassed area. The site is bounded by Whitehall to the north and east which also serves the other Latitude development plots and Whitehall Road to the south and the railway embankment to the west.
- 3.3 The area around the site is evolving and a number of consented/ in construction developments are noted which are Get Living to the south of Whitehall Road which ranges from 8 to 23 storeys, Monkbridge development which ranges from 12 to 22 storeys and the development of Latitude which range from 8 to 31 storeys. The general theme of architecture along Whitehall Road is modern and contemporary, aside from the Monk Bridge Viaduct and Monk Bridge which are both grade II listed.
- 3.4 Pedestrian links are located to the south along Whitehall Road and also along Monk Bridge Viaduct leading in and out of the city centre.
- 3.5 The site forms part of a wider site identified in the Site Allocations Plan (SAP) as a Mixed Use Allocation site ref. MX1-11 for at least 50380sqm office use and 463 Residential units. This allocation involves the other areas to the north and east which have already been developed or are in the process of being developed, which equates

to a site allocation of 3.49 hectares. The entire site is located within Flood Risk Zone 1.

4.0 RELEVANT PLANNING HISTORY:

- 4.1 17/03305/COND - Consent, agreement or approval required by conditions 8, 13, 14, 15, 16, 17, 18, 19, 26, 27, 30, 49 and 50 of Planning Application 16/04118/OT – Split decision – 17.07.2017
- 4.2 13/02017/RM - Reserved matters application for 10 storey office block with basement car park and roof top plant room – Approved – 12.10.2015
- 4.3 16/04118/OT - Amendments to conditions 4 (phasing plan), 46 (Design Statement/Design Code) and 53 (plans schedule) of approval 06/02880/OT – Approved – 21.10.2016
- 4.4 06/02880/OT - Outline application to layout access and erect multi level mixed use development for residential and office uses up to 33 storeys high, with ancillary class A1, A2, A3, A4, A5, D1 and D2 uses and associated car parking and landscaped areas. – Approved – 07.09.2007

5.0 HISTORY OF NEGOTIATIONS:

- 5.1 The proposals have been the subject of pre-application discussions between the Developer, their Architects, and Local Authority Officers since September 2022. The discussions as part of this preapplication focused on the design, massing and layout of the scheme including relationship with Whitehall Road. Extensive wind testing and reviewing of mitigation measures. Highway matters including access and servicing strategy and traffic modelling. Landscaping matters including landscaping strategy, tree planting and amenity spaces.
- 5.2 The preapplication proposals were presented to City Plans Panel on 10th August 2023. Members made the following comments, which are relevant to this aspect of the development; Members were generally supportive of the proposal and welcomed the introduction of the roof terrace and sustainability issues. Members also hoped the proposal would reflect the heritage of the site with recognition to the history of the site.

6.0 PUBLIC/LOCAL RESPONSE:

- 6.1 Planning application publicity consisted of:
Site Notices posted adjacent and around the site on 07.11.2023
Press Notice published 17.11.2023
- 6.2 As part of the application publicity no letters of comment have been received and the publicity period closed on 20.01.2023.
- 6.3 Hunslet & Riverside Ward Councillors have been alerted to this application via email, no responses were received

7.0 CONSULTATION RESPONSES:

- 7.1 **Network Rail** – No objection, conditions recommended.
Response: The required planning conditions will be applied

- 7.2 **West Yorkshire Police** – No objection in principle to the proposal providing that the recommendations are considered and secured via condition as part of any planning approval.
Response: The required planning condition will be applied
- 7.3 **Active Travel England** - ATE recommend any decision on this application is deferred until more information is provided to help address the following issues:
- There is a need to further develop the analysis within the Transport Assessment and crucially the Travel Plan to better reflect active travel journey and the requirement to increase these modes in goals, aims and targets.
 - Better provision for cycle parking on the ground floor and an understand of the security of the short stay spaces is required.
- Response: These matters are being addressed in consultation with Active Travel England but will not affect the overall design and nature of the proposals presented to Panel
- 7.4 **LCC Highways** – see paragraphs 10.34 to 10.41 below
- 7.5 **LCC Influencing Travel Behaviour** - that there is a requirement for the following obligations: A Travel Plan Review fee of £5504 (subject to an annually increase to reflect inflation). A Contribution of £22,000 free trial membership and usage of the car club by office occupiers of the development. The S106 should make clear that if the contribution is not spent on car club trial for the office occupiers, the site wide travel plan co-ordinator can use the contribution for other sustainable travel measures for the employees e.g. public transport tickets, hire bikes, etc.
Responses: These obligations will be secured via the legal agreement.
- 7.6 **LCC Sustainable Development Unit (Climate Change Officer)** - states that the submitted Energy Strategy states its intent to comply with the Leeds Core Strategy Policies EN1 (i), (ii), EN2 and EN4.
Response; to allow these matters to be addressed fully at the appropriate design stage/s these matters will be controlled via Planning Conditions.
- 7.7 **LCC District Heating Network** - This site is not within connection distance of a live heat network. However, the council is working on a large-scale extension to its Leeds PIPES district heating network, the route for which would be close to this site. It is anticipated that on the basis of the council securing sufficient funding, the network will be extended in the next three years, with a rough heat-on date of 2025. It is recommended therefore that this scheme be designed for a connection, ideally at the time of completion or at the very least in the future. In addition, the government's heat network zoning scheme is due to come into effect in 2025, which is likely to mandate connection of schemes such as this to heat networks.
Response: The site features a connection point for future connection to the heat network, which will be secured via condition.
- 7.8 **LCC Environmental Health Services (Pollution Control)** - Having reviewed the proposals and the noise assessment submitted in support of this application, we have no adverse comment to make. Recommendations made in the noise assessment with regard to fixed plant emissions and building design demonstrate that no adverse impacts are likely to arise.
- 7.9 **LCC Sustainability - Design Team** – proposal follows pre application advice. All external materials should be conditioned, and conditions are put into any approval for 1:20 details to secure the reveal depths and the like. We suggest asking for the details through all the major section points through the building. These would include all

openings, parapets, doors and the like. We would suggest a minimum reveal depth of 215mm.

Response: Such conditions will be applied.

- 7.10 **LCC Sustainability – Conservation Team** - state that the proposal does not have any impact on nearby heritage assets and would have a neutral impact upon the character and significance of the Holbeck and Canal Wharf Conservation Areas.
- 7.11 **LCC Sustainability - Nature Team** - state they have no objections to the proposal subject to signing a legal agreement for Biodiversity improvements works off site (£25,000). The shortfall of 0.91 Habitat Units could be delivered on Leeds City Council's or a partner's land in the same locality and secured through the S106 agreement. Conditions are required regarding a Protected Species: Reasonable Avoidance Measures Method Statement, Lighting Design Strategy for Bats and details of integral bat roosting and integral bird nesting features
Response: Such relevant conditions will be applied and the Biodiversity works will be secured via a legal agreement.
- 7.12 **LCC Sustainability - Landscape Team** - state that the details of the landscape scheme need to be addressed via Conditions
Responses: Such relevant conditions will be applied
- 7.13 **LCC Access Officer** - a condition for landscaping should be included which states that it needs to meet BS8300 and a condition is also required for all public seating.
Response: Such condition will be applied.
- 7.14 **LCC Flood Risk Management** - require conditions to cover details of surface water disposal.
Response: Such condition will be applied.
- 7.15 **LCC Environmental Studies – Transport Strategy** - We have no objection to this proposed development, but Environmental Health may wish to comment on the potential issue of commercial noise etc.
- 7.16 **LCC Contaminated Land Team** - state that conditions to cover the submission of a Remediation Strategy is required.
Response: such condition will be applied.
- 7.17 **Yorkshire Water Service Ltd** - require conditions to cover details of surface water disposal.
Response: Such condition will be applied.
- 7.18 **West Yorkshire Archaeological Service** - There are no archaeological implications to the proposed development.
- 7.19 **LCC Local Plans** – No response received
- 7.20 **LCC Cycling Officer** – No response received
- 7.21 **Neighbourhood Forum** - No response received
- 7.22 **Canal & River Trust** – no comments to make on the proposal
- 7.23 **Employment and Skills** - No response received

- 7.24 **Ramblers Association** - No response received
- 7.25 **Health Partnerships** - No response received
- 7.26 **Public Health** - No response received
- 7.27 **Environment Agency** - No response received
- 7.28 **West Yorkshire Ecology** - No response received
- 7.29 **Yorkshire Wildlife Trust** - No response received
- 7.30 **Historic England** – No comments to make
- 7.31 **Natural England Consultation Service** - No response received

8.0 RELEVANT PLANNING POLICIES:

8.1 Statutory Context

- 8.1.1 Section 38(6) of the Planning and Compulsory Purchase Act states that for the purpose of any determination to be made under the Planning Acts, the determination must be made in accordance with the plan, unless material considerations indicate otherwise. The development plan currently comprises the adopted Local Development Framework Core Strategy as amended (2019), those policies saved from the Leeds Unitary Development Plan (Review 2006) (UDP), the Aire Valley Leeds Area Action Plan (2017), the Natural Resources and Waste Local Plan, the Site Allocations Plan (July 2019) and any made Neighbourhood plan.

8.2 Development Plan

- 8.2.1 **Leeds Core Strategy (as amended 2019)** sets out strategic level policies and vision to guide the delivery of development investment decisions and the overall future of the district. Relevant Core Strategy policies will include those outlined below.

Spatial Policy 1 Location and scale of development.

Spatial Policy 2 hierarchy of centres and spatial approach to retailing, offices, intensive leisure and culture

Spatial Policy 3 City Centre Development

Spatial Policy 8 Economic development priorities

Spatial Policy 9 Employment

Spatial Policy 11 Transport infrastructure investment priorities such as pedestrian improvements

Policy CC1 City Centre Development

Policy CC2 City Centre South

Policy CC3 Improving connectivity between the City Centre and Neighbouring Communities.

Policy EC2 Office Development

Policy P8 Sequential and Impact Assessments for Main Town Centre Uses

Policy P10 Design

Policy P11 Heritage

Policy P12 Landscape

Policy T1 Transport Management

Policy T2 Accessibility Requirements and New Development

Policy EN1 Carbon Dioxide Reduction

Policy EN2 Sustainable Design and Construction
Policy EN4 District Heating
Policy EN5 Managing Flood Risk
Policy G8 Protection of Important Species and Habitats
Policy G9 Biodiversity Improvements
Policy ID2 Planning Obligations and Developer Contributions

8.3 Relevant Saved Policies from the **Leeds Unitary Development Plan (UDP)** are:

Policy GP1 - Land use and the Proposals Map
Policy GP5 - General planning considerations.
Policy BD2 – New buildings
Policy BD4 - All mechanical plant
Policy BD5 – Residential amenity
Policy LD1 - Landscape design

8.4 **The Site Allocations Plan** was adopted in July 2019 elements of this were remitted and subsequently adopted in January 2024.

8.4.1 The site and neighboring sites lie within the designated city centre, which are also identified in the Site Allocations Plan (SAP) as a Mixed Use Allocation site ref. MX1-11 for at least 50380sqm office use and 463 residential units.

8.5 The Natural Resources and Waste Local Plan was adopted by Leeds City Council on 16th January 2013 with revised policies Minerals 13 and 14 (Adopted September 2015). The plan sets out where land is needed to enable the City to manage resources, like trees, minerals, energy, waste and water over the next 15 years, and identifies specific actions which will help use natural resources in a more efficient way. Relevant policies include:

GENERAL POLICY1 – Presumption in favour of sustainable development.
AIR1 – The Management of Air Quality through Development measures.
WATER1 – Water efficiency
WATER2 – Protection of Water Quality
WATER7 – No increase in surface water run-off, incorporate SUDs.
LAND1 – Land contamination to be dealt with.¹
LAND2 – Development conserve trees and introduce new tree planting.

8.6 The National Planning Policy Framework (NPPF) was updated in December 2023 and sets out the Government's planning policies for England and how these should be applied (para 1), and is a material consideration in planning decisions (para 2). It states that the purpose of the planning system is to contribute to the achievement of sustainable development (para 7). So that sustainable development is pursued in a positive way at the heart of the Framework is a presumption in favour of sustainable development (paras 10-11). It states that decision makers at every level should seek to approve applications for sustainable development where possible (para 38). The Framework sets policies on the following issues which are relevant to this planning application proposal (including section numbers):

2 Achieving sustainable development (paras 7, 8, 10, 11, 12)
4 Decision making (paras 38, 39, 40, 41,42, 43, 47, 48, 55, 56, 57, 58)
6 Building a strong competitive economy (para 85)
7 Ensuring the vitality of town centres (paras 90, 91)
8 Promoting healthy and safe communities (paras 96, 97, 98,101, 104)
9 Promoting sustainable transport (paras 108,109,112, 115,116,117)

- 11 Making effective use of land (paras 123, 124, 128)
- 12 Achieving well designed places (paras 131, 132,133,135, 136,140)
- 14 Meeting the challenge of climate change and flooding (paras 157-175)
- 15 Conserving and enhancing the natural environment (including ground conditions (paras 180, 185, 186, 188, 189,190,191,192,194)
- 16 Conserving and enhancing the historic environment (paras 200, 201,203)

8.7 Relevant Supplementary Planning Guidance includes:

- SPD Building for Tomorrow Today: Sustainable Design and Construction
- SPD Transport
- SPD Accessible Leeds
- SPD Tall Buildings Design Guide
- SPG City Centre Urban Design Strategy
- SPG Sustainable Drainage in Leeds

9.0 MAIN ISSUES

- Principle of the proposed use
- The impact on the character and appearance of the street scene and wider area
- Impact on future residential amenity
- Landscape proposals
- Biodiversity
- Inclusivity
- Sustainability and Climate Change
- Wind impact
- Highways and transportation considerations
- Air Quality/ Noise
- Safety and security
- Other matters
- S106 legal agreement
- Planning Balance

Principle of the proposed use

- 10.1 The site and wider area is allocated in the Site Allocations Plan (SAP) under site reference MX1-11 for office and residential uses, with a site capacity of 50,380sqm office development and 463 residential units.
- 10.2 This proposal consists of an office use (Class E(g)(i)) and includes an area for a café (Class E(b)) and co working space (Class E(g)(i)) (251m²) which will be open to the public at ground floor. The café/co working space would activate the ground floor frontages, generate footfall, provide vibrancy to the development, and serve nearby residents and other business users in this part of the city. The space would be limited in floorspace and on this basis is not considered to undermine the vitality of the prime shopping area within the city centre; providing a direct and targeted element to support the proposed use in the scheme and vicinity of it. Control of this matter will be addressed by condition which will also serve to prevent the future introduction of uses which could be detrimental to the amenities of the site without further consideration within Class E and may (as a result of the nature of such uses) promote a deadened frontage to the public spaces and/or may have different servicing needs which require further consideration.
- 10.3 The development has been indicated to provide (approximately) 19,236sqm of office accommodation and would involve the regeneration of a site which has laid dormant for

a number of years and now falls within an area of ongoing regeneration. The site falls within the designated city centre and it is considered that the proposed office use would contribute to the ongoing mix of a vibrant community in this area of the City. It is acknowledged that this proposal would not feature any residential accommodation, however residential uses have already been provided to the north of the site which fall within the wider allocation.

The impact on the character and appearance of the street scene and wider area

- 10.4 The proposal seeks to create a 12 storey office building of which the height of the building has been carefully considered and is based on the surrounding context to this area of the city centre. This consideration has also taken into account potential impact on the setting and key views of nearby heritage assets to the east and from within the nearby Holbeck Conservation Area to the south/east of the site. It is considered that the proposed scale of the building would form part of the existing modern character of buildings along the north side of this stretch of Whitehall Road as viewed from within the more historic conservation area and would not impact upon existing heritage assets of which the existing developments to the wider site would shield this building from any views from historic assets. Given this, the buildings height would not unduly dominate the skyline and would be contextual to similar developments within this side of the city centre.
- 10.5 In terms of the urban grain, the footprint of the building remains broadly in the same position as to that approved previously at outline stage and then subsequently approved at reserved matters albeit with alterations to the design in order secure the desired sustainability credentials. The proposal compares favourably to the character of the surrounding street scape. Whilst the site is currently an open cleared area of land, the existing site is uncharacteristic when considering the emerging urban grain of surrounding developments to the north and east. This development would create an infill to existing brownfield land and a new arrangement of building with enhanced pedestrian routes to the west and vehicular access which is intended to sit within the existing street pattern.
- 10.6 The elevational arrangement to this building is highly ordered with regular grid arrangements of brick and different shades, with windows set into reveals with balconies to the northern and southern elevations. The final materials of all built elements will be controlled through conditions which will allow for a finer degree of control. The proposals would be of a high quality design, with detailing and materials that would be appropriate to the wider context, complementing the character and appearance of the surrounding area.

Impact on future residential amenity

- 10.7 Neighbourhoods for Living (NFL) provides general guidance on traditional minimum distances when based in a suburban area, which range from 21m for main living uses to other main living uses and 15m from secondary uses to secondary uses. The explanatory text within NFL also states that these distances are a guide and do not take into account the local context. Given the site is located within Leeds City Centre boundary and the existing tight urban grain, the traditional 21m and 15m separation distances which are applicable to suburban sites would not be appropriate on this site given the context and density of the neighbouring area. There is no specific guidance on relationships between buildings in the city centre and this needs to be based on a contextual approach.

- 10.8 The separation distances of the building from the emerging neighbouring buildings have been assessed. The height of this building (12 storeys) is a lot smaller than the residential blocks (Purple B) consented to the north (17 & 21 storeys). This building would retain at least 14.8m to bedroom uses and 16.4m to main living uses within block b of Purple B to the north, and 23m to the east to the neighbouring office building, 22m would be retained to the Get Living development to the south, and 17m towards the railway embankment at the closest point to the west. These separation distances between commercial uses and residential uses are considered commensurate with the existing and emerging context of the surrounding area of the city centre.

Landscape proposals

- 10.9 A variety of planting will be involved, including environment and setting appropriate species and specimen sizes of trees, shrubs, grasses, wildflowers and border plants, to ensure an attractive, colourful landscape scheme is achieved as part of the development.
- 10.10 As part of the proposal improvements to Whitehall Road would consist of a designated cycle path with bus bypass along with new landscaping to the north, east, south and west of the building. A minimum 2m footway along the eastern and northern side of the site will also be accommodated.
- 10.11 The landscaping proposed to the north consists of shade tolerant planting which will feature climbing plants against the aluminium perforated cladding to the ground floor. The planting to the eastern boundary of the site would create a buffer between the proposed development and Whitehall and would consist of a 1.8m wide pedestrian route, with full shade/ partial shade tolerant planting beds in between with seating for people to dwell and a pedestrian route of 2m wide along the buildings perimeter.
- 10.12 The main access into the building is from the south in which landscaping would consist of tree planting with a mixture of full sun and shade tolerant planting along the frontage to soften the buildings appearance. The planting would also sit alongside the councils improvements works along Whitehall Road, including a bidirectional cycling lane, footway improvements and bus stop bypass.
- 10.13 Vehicular access into the site is from Whitehall and leads into the undercroft car park to the west of the building. Beyond the car park an area of landscaping is proposed which will feature tree planting, areas of seating and an improved north/south pedestrian link and planting of both shade tolerant and full sun tolerant planting, depending on the location.
- 10.14 Comments raised by Members at the 10th August 2023 pre application presentation related to the heritage of the site and how this could be incorporated into the final scheme. In response to these comments, the applicant has confirmed that the approach to the site will consider the historical use of the site and will draw inspiration and reference to the masonry firing hearths, the cart tracks and iron Ingot through the shapes and areas proposed within the hard landscaped area. These features are to be located within the western landscaped area and details of the interpretation including signs about the history of the site will be conditioned as part of any approval.
- 10.15 The roof terrace of the building creates a private space for employees and a breakout space. The terrace would feature a green wall with areas of landscaping within planting beds providing opportunities for recreation, gathering, socialising and potential group activities.

- 10.16 The soft landscaping proposals will enhance the building base and the external green environment with the inclusion of low level planting which will provide a green carpet supported by additional planting specifically chosen to mature in shaded settings. The perforated screens at ground and first floor levels are softened with climber plants, adding interest and biodiversity immediately at street level.
- 10.17 The areas of landscaping will be managed, and the emerging design and layout offer natural surveillance of the external landscaped areas. Full details of all soft and hard landscape proposals and their maintenance will be required to come forward under planning conditions.

Biodiversity

- 10.18 Core Strategy policy G9 states that developments will need to demonstrate: (i) That there will be an overall net gain for biodiversity commensurate with the scale of the development, including a positive contribution to the habitat network through habitat protection, creation and enhancement, and (ii) The design of new development, including landscape, enhances existing wildlife habitats and provides new areas and opportunities for wildlife, and (iii) That there is no significant adverse impact on the integrity and connectivity of the Leeds Habitat Network.
- 10.19 With regards to Biodiversity, it is noted that the Baseline is 1.33 Habitat Biodiversity Units (lost) and that through landscaping there should be approx. 0.91 Habitat Biodiversity Units delivered on-site. Given this the scheme does not achieve a measurable Biodiversity Net Gain (BNG). To achieve Leeds City Council's aims of a 10% BNG which is soon to be mandated through the section 90A of the Town and Country Planning Act 1990 as amended by the Environment Act 2021. Leeds City Council requires 0.91 Habitat Units. The BNG Assessment states the developer intends to secure these units on Leeds City Council land. In accordance with Leeds City Council guidance, the cost of one Habitat Unit would be £25,000 and would be used towards biodiversity projects/measures which would be delivered in the same ward (or immediately adjacent to the ward subject to agreement of the Local Planning Authority).
- 10.20 Given the above the proposal is acceptable to the Nature Team subject to the signing of the S106 agreement and the insertion of conditions controlling details of the biodiversity enhancement measures on site.

Inclusivity

- 10.21 The Equality Act 2010 requires local authorities to comply with the Public Sector Equality Duty. Taking into account all known factors and considerations, the requirement to consider, and have due regard to, the needs of diverse groups to eliminate discrimination, advance equality of opportunity and access, and foster good relations between different groups in the community has been fully taken into account in the consideration of the planning application to date and at the time of making the recommendation in this report.
- 10.22 In this instance it is considered that the proposals do not raise any specific implications in these respects and therefore it is not considered that a full Equality, Diversity, Cohesion and Integration Impact Assessment (EDCI) is required.
- 10.23 The building would feature fully accessible entrances and lift access would be provided to all floors. The developer has confirmed the development would provide accessible accommodation in line with the Accessible Leeds SPD, BS8300 and Building

Regulations standards and on this basis, subject to a condition covering this the proposal is considered acceptable and would comply with The Equality Act 2010.

Sustainability and Climate Change

- 10.24 Members will be aware that the Council has declared a Climate Emergency. Existing planning policies seek to address the issue of climate change by ensuring that development proposals incorporate measures to reduce the impact of non-renewable resources.
- 10.25 The proposal will introduce a number of measures to ensure that Core Strategy policy EN1 (Climate Change – Carbon Dioxide Reduction) is complied with. The developer's accompanying sustainability statement confirms that reduction in energy use is achieved through Air Source Heat Pumps (ASHPs) and Photovoltaics (PV) which are to be implemented into the design of this development:
- 10.26 The above measures will ensure a minimum of 20% reduction in carbon dioxide emissions is achieved (23%) against the Building Regulations Target Emission Rate Part L 2021 and energy reduction through the use of renewable energy generation would meet the 10% (59%) figure as set out in policy EN1.
- 10.27 In addition to the above, early evidence supplied has stated that the development will achieve a BREEAM standard of 'outstanding' which exceeds the requirement of Policy EN2, which requires an BREEAM standard of 'excellent'. To ensure this The Design stage BREEAM certification will need to be provided within 3 months of commencement of construction and the final BREEAM certification should be provided within 12 months of occupation. These details will be secured via condition.
- 10.28 In regard Policy EN4, the proposal is currently not located close to the District Heating Network but there are plans to extend the district heat network to the South Bank in the next 3-5 years, and the eventual pipe route would likely run right past this site. Due to the uncertainty of this proposed extension the applicant has agreed to revisit the potential to connecting to the District Heating network at discharge of condition stage. This said, the site layout has now been annotated with a District Heating Room location and the Council's Sustainability Officer has now accepted this position and the control of this matter via condition.
- 10.29 It is further noted that the additional tree planting, improved accessibility and the introduction of electric vehicle charging points within the car parking areas of the site, will also assist in tackling climate change and air pollution in line with wider Council objectives and assist in encouraging more sustainable travel choices.

Wind impact

- 10.30 A wind study was submitted as part of the application which considers the impact on the prevailing winds as a result of the development/proposed developments and any creation of localised wind focused micro-climates. The Lawson distress criteria is an industry standard and as such this has been employed in the wind study. This criteria states that a wind speed greater than 15 metres-per-second occurring for more than two hours per year is classified as unsuitable for the general public and represents a wind speed with the potential to destabilise the less able members of the public such as the elderly, cyclists and children. Able-bodied users are those determined to experience distress when the wind speed exceeds 20 metres-per-second for two hours per year.

- 10.31 The wind study has been peer reviewed and it has been demonstrated that there are wind safety exceedances present across the Latitude Yellow site and neighbouring Purple B and Red sites, in both the current and cumulative surrounds scenarios. The most significant of these are to the SW of Purple B where exceedances are found at more than 10 locations, with durations of up to 26 hrs/yr, against a Lawson threshold of 2 hrs/yr. The modellers have shown that construction of the proposed Latitude Yellow building, including its “builtin” wind mitigation measures (screens, canopies etc.) and one offsite screen adjacent to the Get Living development (19/03109/FU), is effective in removing most of these wind safety issues, in both the existing and cumulative surrounds scenarios, and in significantly reducing the size and duration of the exceedances that it cannot remove.
- 10.32 The risk to pedestrians on the Latitude Purple B site from these residual wind exceedances (i.e. post construction of Latitude Yellow) is classed as acceptable by the independent peer reviewer due to the limited size and duration of the hazard and the conservative approach to the wind modelling work which does not take into account the likely mitigation effect of the landscaping features approved on the under construction Latitude Purple B site. Results were also produced for the previous, consented design of the Latitude Yellow building, which demonstrated that wind conditions are better with the newly proposed scheme than they are in the consented scheme.
- 10.33 The wind study has been peer reviewed and is considered to be robust and of a suitable quality and to provide realistic result, consistent with the reviewer’s expectations and no recommendations are requested.

Highways and Transportation Considerations

- 10.34 The site is located in a highly sustainable city centre location. The proposal includes 10 car parking spaces (including 2 disabled spaces). In addition, 5 motorcycle parking spaces are also proposed. Given the sustainable location of the development, this provision is acceptable and raises no concerns.
- 10.35 The applicant proposes Electric Vehicle Charging Points (EVCP) to all car parking spaces in line with the standards set out within CS Policy EN8. The specification for the EVCPs will be secured via planning condition.
- 10.36 The development would also provide 138 long stay cycling spaces for employees which will be split between the ground floor level and mezzanine level and comprise of Sheffield Stands, Double Racks and Brompton Lockers. Confirmation has also been provided regarding how cyclists will access the mezzanine level, which will be via an enlarged 1.2m wide staircase with a wheel ramp. In addition, there would also be 20 short stay spaces provided through 10 Sheffield stands within the landscaped area, close to the main entrance which will be provided with natural surveillance.
- 10.37 Servicing and deliveries are proposed to operate via the access road to the east, which also serves the other developments. Final details of this will be secured via conditions for a car park, servicing, and delivery management plan.
- 10.38 The scheme also incorporates a new bidirectional cycleway to the north side of Whitehall Road, including a bus stop bypass. This cycleway will link into the wider cycle improvement scheme by The Council which is proposed along Whitehall Road.
- 10.39 In the scenario that this development is built along with the consented Get Living scheme (19/03109/FU) two wind safety exceedances are created within the carriageway of Whitehall Road. Whilst the risk to pedestrians could be reduced by

introducing guard rails in that vicinity to prevent pedestrians from crossing the highway, the Local Planning Authority (LPA) wouldn't be able to prevent cyclists from using the area of exceedance, especially if they were overtaking a bus at the stop. Given this a wind mitigation screen has been proposed to the southern side of Whitehall Road, which removes these exceedances. The screen would be 25m long with 20m located at ground level and then 5m located 2.6m above ground level. At the time of finalising this report, a meeting has been scheduled with highways and planning to discuss the acceptability of this screen within the adopted highway and in the event that 19/03109/FU is consented. Members will be updated verbally on this element and this discussion does not affect the overall proposal which is to be presented to Members.

- 10.40 As a result of this development, there will be a direct impact on the Globe Road/Whitehall Road Junction. There is an identified scheme to improve the Globe Road/Whitehall Road Junction, forming part of the city centre Package, therefore a contribution is required towards this scheme, commensurate with the size of the development. The cost of the junction works are conservatively estimated at £1.365M. Taking contributions from other developments into account, the contribution required is £188,250.00. This contribution has been agreed to and will be secured via the legal agreement.
- 10.41 In summary, the proposal would have an appropriate level of parking, make appropriate transportation provision, promote sustainable travel and improved connectivity and would deliver cycle improvements along Whitehall Road and improved pedestrian links to Whitehall, through a new public realm space, and would not be likely to give rise to adverse parking, road safety or amenity concerns.

Air Quality/ Noise

- 10.42 An Air Quality Assessment has been carried out by the applicant which confirms impact on air quality standards would be negligible at the site or elsewhere as a result of the proposed development.
- 10.43 During the determination of the application a Noise Assessment has been submitted which Environmental Health have been consulted on and agree with their methodology and criteria adopted. Noise limits for new fixed plant are also acceptable and it is recommended that final details of plant are also secured via condition. In addition, a further condition is also recommended limiting construction hours from 0800-1800 Monday to Friday and 0800 to 1400 on Saturdays with no working on Sundays or Bank Holidays. In addition, a condition is also recommended restricting the use of the external roof top area after 11pm, 7 days a week, given the evolving character to this area of the city and to protect future residential amenity.

Safety and Security

- 10.44 The Applicant has provided the following statement on safety and security; *The building has been designed to be as passively secure as possible. The ground floor layout affords good levels of natural surveillance in and around primary building access points. These are supported by full site CCTV coverage ensuring that anti-social behaviours or loitering are discouraged. Planted greenery around the building is kept purposefully low so as not to hinder surveillance. The main reception access doors will permit free access into a reception space which will be monitored. Other access points will be equipped with suitable alarm detection and CCTV coverage. A dedicated building manager's room will provide a place for continuous building security monitoring. The building structure and low-level planting structures could be considered to provide a degree of protection against vehicle ramming. Further work*

may be required to develop this strategy. The building also proposes to follow the principles of secured by design without certification. Ground floor glazing and door specification will require to provide suitable and appropriate levels of crime protection. It is not envisaged that an external party would rent or occupy the level 12 terrace space. Inviting un-solicited persons into the building and onto this level may be seen as conflicting with the rest of the building's safety and security strategy.

- 10.45 To ensure the above measures are implemented conditions are recommended securing details of all security measures and external lighting strategy. It is considered that the activation of this site would have a benefit to the people using this site/walking by. The development will provide active frontages at ground floor and an increase in pedestrian movements through and around the site, therefore a greater amount of natural surveillance and lighting, when compared to the open land site as it stands today.

Other matters

- 10.46 In respect of drainage matters, the application site lies within Flood Zone 1 and there have been no records of any recent flooding within the site or adjacent areas with a low probability to flood. The development has been accompanied with a Drainage Strategy which requires amendments, however given these matters are technical Yorkshire Water and LCC Flood Risk Management are comfortable that these matters can be dealt with via conditions.
- 10.47 Regarding land contamination matters, a site investigation report has recently been submitted which provides additional soil and soil leachate analysis and assessment, ground gas monitoring and assessment. It also confirms that gas protection including for vapours is required, and a cover layer in areas of soft landscape. This report also notes that remedial measures will be finalised by designers and as such doesn't include a soil import strategy or ground gas protection verification plan. Given this the following will be required to be submitted prior to development commencing and will be secured via condition, a soil import strategy and a ground gas protection verification plan.

S106 legal agreement

- 10.48 Policy ID2 of the Core Strategy (as amended) is regarding planning obligations and developer contributions, which states that section 106 planning obligations will be required as part of a planning permission where this is necessary, directly related to the development, and reasonably related in scale and kind in order to make a specific development acceptable and where a planning condition would not be effective. This is further enforced by the Community Infrastructure Levy Regulations 2010. which provide that a planning obligation may only constitute a reason for granting planning permission for the development if the obligation is -

- (a) Necessary to make the development acceptable in planning terms,
- (b) Directly related to the development; and
- (c) Fairly and reasonably related in scale and kind to the development.

- 10.49 The Council's adopted policies would result in a requirement for the following obligations;

- 0.91 Biodiversity Units to be Habitat Units delivered off site in the same locality – £25,000
- On site Public Realm areas are publicly accessible

- Travel Plan review fee – £5504
- Contribution for free trial membership and usage of the car club by occupiers of the development and/or other sustainable travel measures for the employees e.g. public transport tickets, hire bikes – £22,000
- Globe Road/Whitehall Road Junction improvements – £188,250
- Management Plan to cover maintenance of Whitehall
- Wayfinding – £16,000
- Employment and training of local people
- Monitoring fees.

10.50 The development is Community Infrastructure Levy (CIL) liable and is estimated to be in the region of £921,368.92. CIL is generally payable on the commencement of development. The CIL contributions are spent on infrastructure projects to meet the needs created by new growth.

PLANNING BALANCE

10.51 The scale and massing of the development is considered wholly appropriate within this dense city centre environment. The identification of this site and wider area within the Site Allocations Plan (SAP) recognised this area of the city for development. The proposed development follows similar principles already established as part of the original Masterplan of this site, which were originally approved in 2006 and have been implemented.

10.52 The proposal would help to further transform a key highly prominent brownfield city centre site, which will complete the final piece in this allocated site. The proposal would also help to regenerate this area of the city in a highly sustainable location and improve/enhance landscaping & connectivity within the city centre and also provide additional facilities to nearby neighbourhoods. The development would also propose all S106 obligations and these matters are given substantial weight in favour of the development.

11.0 CONCLUSION

11.1 In conclusion, the development would involve the further regeneration of a key brownfield allocated site within the city centre boundary. The site is in a highly sustainable location, 20-minute walk to Leeds train station and within easy access to numerous bus stops. The development of this site would also contribute to the mix of developments within this area of the city centre and would also introduce new employment, public landscaped areas, improved pedestrian and cycle connections and add to the vibrancy and vitality to the area and further the regeneration to this part of the city centre. The development of this site would also follow similar design principles already established as part of the overall Masterplan for this site, approved in 2006 originally.

11.2 Therefore, on balance the proposal is considered to be in accordance with the Development Plan and is considered to be acceptable and is therefore recommended for approval, subject to the conditions set out in Appendix 1 and the signing of a legal agreement to secure the obligations listed in paragraph 10.49.

Background Papers:

23/06266/FU

PREAPP/22/00095

Appendix 1

- 1) The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Imposed pursuant to the provisions of Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

- 2) The development hereby permitted shall be carried out in accordance with the approved plans listed in the Plans and Specifications above.

For the avoidance of doubt and in the interests of proper planning.

- 3) Prior to the installation of any external facing material to the proposed building, full details including a sample panel of the relevant external facing materials, roofing and full details of glazing types shall be constructed on-site and approved in writing by the Local

Planning Authority. The external facing materials, roofing and glazing materials shall be constructed in strict accordance with the sample panel. The sample panel shall not be demolished prior to the completion of the development, unless otherwise agreed in writing by the Local Planning Authority.

In the interests of visual amenity.

- 4) Prior to the construction of the following elements of the proposed building, full 1 to 20 scale working drawing details of the following shall be submitted to and approved in writing by the Local Planning Authority:
- a. soffit, roof line, eaves and any external plant area treatments.
 - b. junctions between materials.
 - c. each type of window bay proposed with a minimum reveal depth of 215mm.
 - d. ground floor frontages.
 - e. ventilation grilles

Development shall then be undertaken in accordance with the approved details.

In the interests of visual amenity.

- 5) Prior to first occupation details of both hard and soft landscaping works, including an implementation programme, shall be submitted to and approved in writing by the Local Planning Authority. Details shall include dimensions, proposed finished levels/gradients and sections/elevations identifying construction standards, material dimensions/types, preservative treatments, below ground foundations, colours, textures etc. associated with.

Hard landscape works shall include:

- (a) proposed finished levels and/or contours,
- (b) boundary details, means of enclosure and retaining structures,
- (c) road/pedestrian/cycling paving areas, steps and ramps
- (d) minor artefacts and structures (eg, tree pits in hard paving, raised planting beds, flush planting beds, refuse/cycle storage units, screening, play features, interpretation features, bins, bollards, lights, paved service covers, linear drainage covers, etc.)
- (e) proposed and existing functional services above and below ground (eg. drainage, sewers, power cables, communication cables, pipelines etc., indicating lines, manholes, supports etc.).
- (f) seating (must meet the full accessibility standard set out in British Standard BS8300, and no seating is to include gaps between arm and back rests unless otherwise agreed in writing by the Local Planning Authority).
- (g) details of contrast nosings

Soft landscape works shall include:

- (h) detailed planting plans including the locations of trees, shrubs, grasses, bulbs, hedge planting etc.
- (i) written specifications (including cultivation, stock handling/quality, seasonal/weather restrictions, planting methods, soil amelioration, drainage, irrigation, surface finish and other B.S compliant operations associated with tree, plant and grass establishment);
- (j) schedules of plants noting species, planting sizes and proposed numbers/densities,
- (k) an implementation schedule.
- (l) details of green wall planting

All Soft Landscaping works to be carried out in accordance with Planting checklist (leeds.gov.uk). Hard and Soft works shall be carried out in accordance with the approved details; approved implementation programme and BS 4428:1989 Code of Practice for General Landscape Operations. The developer shall complete the approved landscaping works and confirm this in writing to the Local Planning Authority prior to the date agreed in the implementation programme.

To ensure the provision and establishment of acceptable landscaping.

- 6) Hard and soft landscaping works shall be fully carried out in accordance with the approved details prior to the occupation of any part of the development in accordance with the programme agreed with the Local Planning Authority and to a reasonable standard in accordance with the relevant provisions of appropriate British Standards or other recognised codes of good practice.

To ensure the provision, establishment and maintenance to a reasonable standard of landscaping in accordance with the approved proposals.

- 7) If within a period of five years from the date of planting, any tree/hedge/shrub dies or is removed, uprooted or destroyed, another tree/hedge/shrub of the same species and size as that originally planted shall be planted in the same location as soon as reasonably possible and no later than the first available planting season, unless otherwise agreed in writing by the Local Planning Authority.

To ensure maintenance of a healthy landscape scheme,

- 8) A LANDSCAPE MANAGEMENT PLAN that applies for the lifetime of the development. The first five years should be detailed more fully to cover an extended defects period for the successful establishment of the proposed landscape. The report should include long term design objectives, management responsibilities and maintenance schedules shall be submitted to and approved in writing by the Local Planning Authority prior to the occupation of the development.

The Management Plan must be a standalone, self-contained document that is not reliant on cross referencing additional information/site plans. The document must therefore include copies of all approved landscape plans and specifications as appendices. All in accordance with Planning Guidance No. 2 LANDSCAPE MANAGEMENT PLANS. Ref: <https://www.leeds.gov.uk/docs/Landscape%20management%20plans%20guidance.pdf>. The landscape management plan shall be submitted to, and approved in writing, prior to the occupation of the development and carried out as approved.

To ensure successful aftercare of landscaping.

- 9) Where trees and shrubs are to be planted adjacent to the railway boundary, they should be positioned at a minimum distance greater than their height at maturity from the boundary. Certain broad leaf deciduous species should not be planted adjacent to the railway boundary. Any hedge planted adjacent to the railway boundary fencing for screening purposes should be placed so that when fully grown it does not damage the fencing, provide a means of scaling it, or prevent Network Rail from maintaining its boundary fencing. Below is a list of species that are acceptable and unacceptable for planting in proximity to the railway boundary;

Acceptable: Birch (Betula), Crab Apple (Malus Sylvestris), Field Maple (Acer Campestre), Bird Cherry (Prunus Padus), Wild Pear (Pyrus Communis), Fir Trees - Pines

(Pinus), Hawthorn (Cretaeagus), Mountain Ash - Whitebeams (Sorbus), False Acacia (Robinia), Willow Shrubs (Shrubby Salix), Thuja Plicatat "Zebrina"

Not Acceptable: Acer (Acer pseudoplatanus), Aspen - Poplar (Populus), Small-leaved Lime (Tilia Cordata), Sycamore - Norway Maple (Acer), Horse Chestnut (Aesculus Hippocastanum), Sweet Chestnut (Castanea Sativa), Ash (Fraxinus excelsior), Black poplar (Populus nigra var, betulifolia), Lomba

In order to safeguard the safety and operational effectiveness of the railway

- 10) Prior to the installation of external lighting adjacent to the railway line details of the lighting should be submitted to and approved in writing by the Local Planning Authority following consultation with Network Rail.

To safeguard the safety and efficiency of the railway

- 11) No works to or removal of hedgerows, trees or shrubs, or built structures with birdnesting potential shall take place between 1st March and 31st August inclusive, unless a competent ecologist has undertaken a careful, detailed check of vegetation or built structures for active birds' nests immediately before (within 24 hours) the works commence and provided written confirmation that no birds will be harmed and/or that there are appropriate measures in place to protect nesting bird interest on site. Any such written confirmation should be submitted to the local planning authority within 3 working days of such works commencing.

To protect nesting birds in vegetation and built structures in accordance with the Wildlife & Countryside Act 1981 (as amended) and BS 42020:2013.

- 12) Prior to commencement of development a Protected Species: Reasonable Avoidance Measures Method Statement (PS:RAMMS) for Bats shall be produced by an appropriately qualified ecological consultant and submitted to and approved by the local planning authority. The PS:RAMMS will include the following:
 - a) Identification of those areas/features on site that have the potential to support Bats using appropriately scaled maps to show where these areas are.
 - b) Timing of works to avoid harm to Bats
 - c) Working methods to avoid harm to Bats
 - d) Person responsible for implementing the PS:RAMMS
 - e) The times during site clearance and construction when specialist ecologists need to be present on site to oversee works
 - f) The role of a responsible person (Ecological Clerk of Works) and lines of communication

The approved PS:RAMMS will be implemented in full accordance with the approved details.

To provide assurance a legally protected species will not be affected

- 13) Prior to any above ground level works, a Lighting Design Strategy For Bats shall be produced by an appropriately qualified ecological consultant and submitted to and approved in writing by the local planning authority. The Strategy shall:
 - a) Identify those areas/features on site that are particularly sensitive for commuting and foraging bats - using an appropriately scaled map to show where these areas are

b) Show how and where external lighting will be installed (through the provision of appropriate lighting contour plans and technical specifications) so that it can be clearly demonstrated that areas to be lit will not disturb commuting and foraging bats.

All external lighting shall be installed in accordance with the specifications and locations set out in the Strategy, and shall be maintained thereafter in accordance with the Strategy. Under no circumstances should any additional external lighting be installed without prior consent from the local planning authority in the areas identified in the Strategy as "particularly sensitive for roosting, commuting or foraging bats".

To safeguard a protected species (bats).

- 14) Prior to any above ground level works, a Plan shall be submitted to and approved in writing by the local planning authority of: integral bat roosting and integral bird nesting features (for species such as House Sparrow and Swift) within the building. Features that are not integral will only be considered for approval if an appropriately qualified ecologist provides assurance that, following discussions with the building architect, integral features are not possible. The agreed Plan shall show the number, specification of the bird nesting and bat roosting features and where they will be located, together with a timetable for implementation and commitment to being installed under the instruction of an appropriately qualified bat consultant. All approved features shall be installed prior to first occupation of the building and retained in the manner as approved thereafter.

To maintain and enhance biodiversity

- 15) Prior to first occupation of the building, written confirmation of integral bat roosting and/or integral bird nesting features will be submitted and approved in writing by the local planning authority. This should include photographs of features in-situ and a written statement that all features have been installed as per the agreed specifications and locations.

To maintain and enhance biodiversity.

- 16) The disabled parking shown on the approved plans shall be laid out prior to first occupation of the development and retained for the life of the development.

In accordance with the adopted Core Strategy and parking policies.

- 17) Means of vehicular access to and from the site shall only be as shown on the approved plan ref TBC and delivered prior to first occupation and retained thereafter for the lifetime of the proposed development.

To ensure the free and safe use of the highway

- 18) Notwithstanding the approved details, works above the ground floor slab level shall not commence until full details of cycle/motorcycle parking and facilities have been submitted to and approved in writing by the Local Planning Authority. The approved cycle/motorcycle parking and facilities shall be provided prior to first occupation of the development and retained thereafter for the lifetime of the development.

In the interests of highway safety and promoting sustainable travel opportunities.

- 19) Development shall not be occupied until a plan showing details of dedicated space for loading, unloading and parking of service/delivery vehicles has been submitted to and approved in writing by the Local Planning Authority. This space shall be retained for the lifetime of the development.

To ensure the free and safe use of the highway.

- 20) Development shall not be occupied until a Car Park and Servicing Management Plan (including timescales and detailed loading bay proposals) have been submitted to and approved in writing by the Local Planning Authority. The plan shall be fully implemented, and the development thereafter operated in accordance with the approved timescales.

To ensure the free and safe use of the highway.

- 21) Prior to occupation of the development, the off-site highway works as shown on plan IN comprising TBC at location TBC shall be fully delivered.

To ensure the free and safe use of the highway

- 22) Development shall not be occupied until Electric Vehicle Charging Points have been provided in accordance with a scheme that shall have been submitted to and approved in writing by the Local Planning Authority. The approved facilities shall thereafter be retained for the lifetime of the development.

In the interest of promoting low carbon transport

- 23) Development shall not commence until a statement of construction practice has been submitted to and approved in writing by the Local Planning Authority, the statement of construction practice shall include full details of:
- a) the construction vehicle routing, means of access, location of site compound, storage and parking (including workforce parking), means of loading and unloading of all contractors' plant, equipment, materials and vehicles and associated traffic management measures.
 - b) methods to prevent mud, grit and dirt being carried on to the public highway from the development hereby approved.
 - c) measures to control the emissions of dust and dirt during construction.
 - d) how the statement of construction practice will be made publicly available by the developer.

The approved details shall be implemented at the commencement of works on site and shall thereafter be retained and employed until completion of the works on site. The Statement on Construction practice shall be made publicly available for the lifetime of the construction phase of the development in accordance with the approved method of publicity.

The carrying out of the development could result in significant harm to the amenities of local residents and highway safety, and accordingly details of construction practice is required to be agreed prior to commencement of works in order to protect such interests.

- 24) Development shall not commence until a construction methodology has been submitted to and approved in writing by the Local Authority. The construction methodology shall demonstrate consultation with the Asset Protection Project Manager at Network Rail. The development shall thereafter be carried out in accordance with the approved

construction methodology unless otherwise agreed in writing by the Local Planning Authority.

The safety, operational needs and integrity of the railway.

- 25) The hours of construction shall be restricted to 08.00 - 18:00 hours Monday to Saturday, with no works on Sundays or Bank Holidays, unless agreed in writing with the Local Planning Authority.

In the interests of residential amenity.

- 26) Within 24 months of the completion of the installation of external facing materials as hereby approved (such a date as to be notified to the Local Planning Authority) in the event of any complaint to the Local Planning Authority from Network Rail relating to signal sighting safety or driver distraction, upon notification to the Local Planning Authority , the applicant shall as soon as possible and not later than 28 days, submit for approval to the Local Planning Authority details of a scheme of remedial measures to address the concerns raised with details of a timescale for implementation of the works. The works shall be carried out in accordance with the approved details and timetable.

To ensure safety of the users of the railway.

- 27) The site shall be developed with separate systems of drainage for foul and surface water on and off site, unless otherwise agreed in writing by the Local Planning Authority. The separate systems should extend to the points of discharge to be agreed.

In the interest of satisfactory and sustainable drainage

- 28) There shall be no piped discharge of surface water from the development prior to the completion of surface water drainage works, details of which shall be submitted to and approved by the Local Planning Authority. If discharge to public sewer is proposed, the information shall include, but not be exclusive to:
- i) evidence that other means of surface water drainage have been properly considered and why they have been discounted; and
 - ii) the means of discharging to the public sewer network at a rate not to exceed 3.5 litres per second

To ensure that no surface water discharges take place until proper provision has been made for its disposal

- 29) The approved Phase II Site Investigation report ('Geo-Environmental Development Appraisal' for Latitude Yellow, Woolgar Hunter, 23/11/2023, 20226001 v1.11) indicates that remediation is necessary, and therefore development (excluding demolition) shall not commence until a Remediation Strategy has been submitted to, and approved in writing by, the Local Planning Authority.

The Remediation Strategy shall include a soil import methodology, a ground gas protection verification plan including detailed design of protection measures, a programme for all works, and allow for the provision of Verification Reports. It is strongly recommended that all reports are prepared and approved by a suitably qualified and competent person.

To ensure that the site is 'suitable for use' with respect to land contamination.

- 30) If remediation is unable to proceed in accordance with the approved Remediation Strategy, or where significant unexpected contamination is encountered, or where soil or soil forming material is being imported to site, the Local Planning Authority shall be notified in writing immediately and operations on the affected part of the site shall cease. The affected part of the site shall be agreed with the Local Planning Authority in writing. An amended or new Remediation Strategy and/or Soil Importation Strategy shall be submitted to and approved in writing by the Local Planning Authority prior to any further remediation works which shall thereafter be carried out in accordance with the revised approved Strategy. Prior to the site being brought into use, where significant unexpected contamination is not encountered, the Local Planning Authority shall be notified in writing of such.

It is strongly recommended that all reports are prepared and approved by a suitably qualified and competent person.

To ensure that any necessary remediation works are identified to make the site 'suitable for use' with respect to land contamination.

- 31) Remediation works shall be carried out in accordance with the approved Remediation Strategy. On completion of those works, the Verification Report(s) shall be submitted to the Local Planning Authority in accordance with the approved programme. The site or phase of a site shall not be brought into use until such time as all verification information has been approved in writing by the Local Planning Authority.

It is strongly recommended that all reports are prepared and approved by a suitably qualified and competent person.

To ensure that the remediation works are fully implemented as agreed and the site has been demonstrated to be 'suitable for use' with respect to land contamination.

- 32) Prior to the commencement of any above ground works an updated Sustainability Statement shall be submitted to and approved in writing by the Local Planning Authority, which will include a detailed scheme comprising:

- a) Policy EN1(i) SAP/ SBEM/ BRUKL report
- b) Policy EN1(ii) calculation of the building's energy demand with what percent of it is being generated by low and zero carbon sources. Details of specifications and locations of all Low or Zero Carbon (LZC) energy sources on site are also required.
- c) Policy EN2 BREEAM certification.
- d) Policy EN4 evidence for one of the four policy options needs to be provided.

The development shall be carried out in accordance with the approved details and

- e) Within 12 months of the final occupation a post-construction review statement shall be submitted to the Local Planning Authority including demonstration that the building(s) have achieved the relevant standards. The development and buildings comprised therein shall be maintained thereafter and any repairs shall be carried out all in accordance with the approved detailed scheme and post-completion review statement.

In the interests of ensuring the development meets the requirements of the adopted energy policies within the Core Strategy.

- 33) The commercial ground floor use shall be used as Class E(a) and/or Class E(b) and/or Class E(g)(i) and shall not be used for any other purposes within Class E or other Use Classes of the Schedule of The Town and Country Planning (Use Classes)

(Amendment) (England) Regulations 2020, or in any provision equivalent to that Class in any Statutory Instrument revoking or re-enacting that Order with or without modification.

As other uses may conflict with the requirements of the adopted development plan and have highway safety implications.

34) There shall be:

No speakers for the playing of amplified music or sound shall be installed on the roof terraces and there shall be no use of the commercial roof terrace after 11pm (except for smoking).

To protect residential amenity given the evolving character to this area of the city centre.

35) Prior to the first use of the ground floor cafe/workspace area, details of a signage/window manifestation strategy shall be submitted to and approved in writing by the Local Planning Authority. This shall set out signage zones on the building and shall at no time be solidly obscured or screened to prevent vision into and through the windows

In the interest of ensuring active frontages and natural surveillance are maintained.

36) Notwithstanding the plans hereby approved, prior to installation of the agreed on site wind mitigation measures as referenced in approved plans: PROPOSED WIND MITIGATION - LOW LEVEL ref: LYPRS-CCA-ZZ-XX-D-A-05023 REV P05, PROPOSED WIND MITIGATION - HIGH LEVEL ref: LYPRS-CCA-ZZ-XX-D-A-05024 REV P05 full design details, including dimensions, exact positions, porosity, materials and fixing details of the wind mitigation measures shall be submitted to and approved in writing by the Local Planning Authority. The approved measures shall be erected prior to first occupation of the development and retained for the lifetime of the development.

In the interests of public safety

37) Notwithstanding the plans hereby approved and in the event that the offsite wind mitigation as referenced in approved plan LYPRS-CCA-ZZ-XX-D-A-05023 S5 P05 as "TYPE 5" is required as a result of the construction of 19/03109/FU details shall be submitted to and approved in writing by the Local Planning Authority, which shall include the full design specification, porosity, and material of the wind mitigation. The development shall then be undertaken in accordance with the approved details.

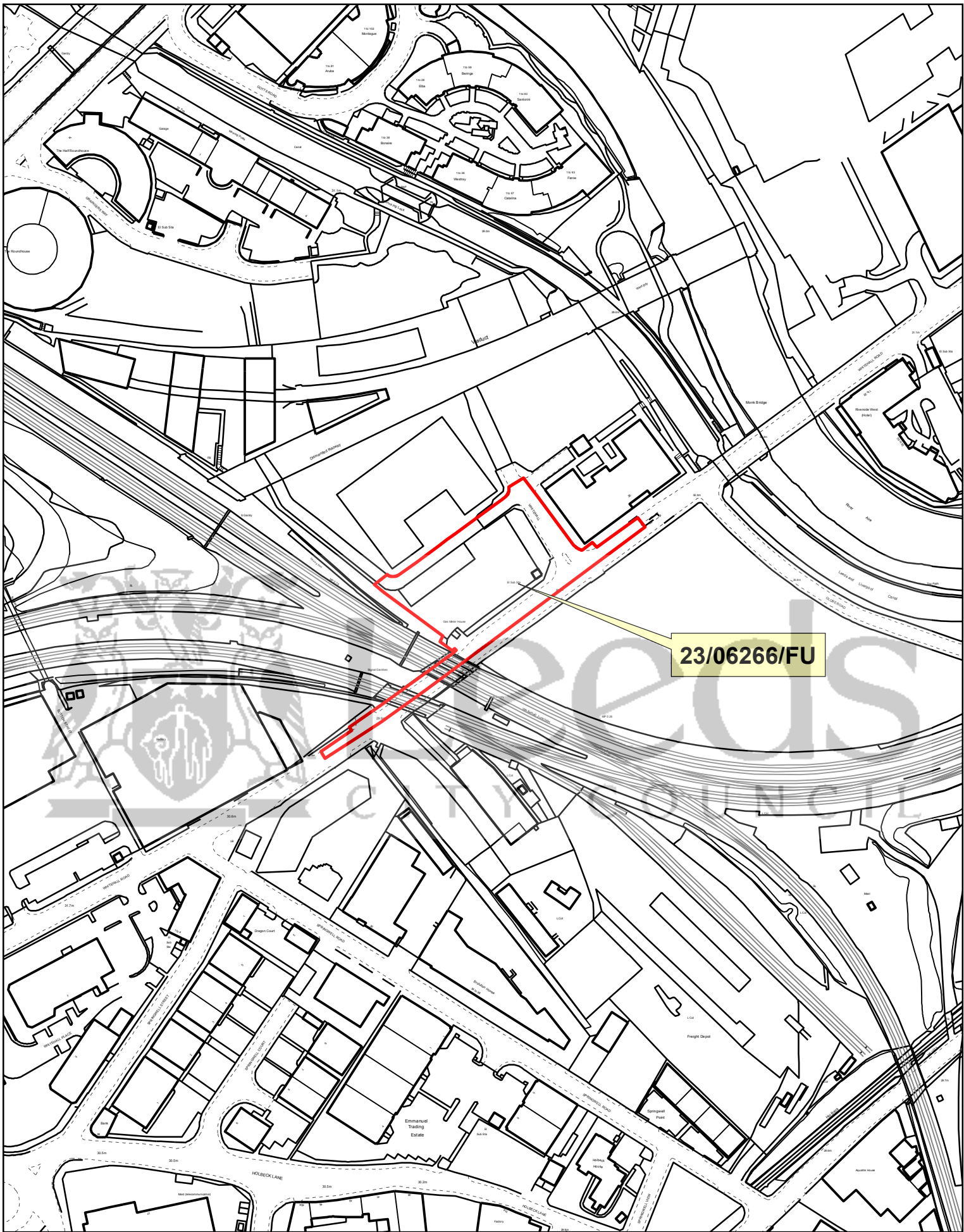
In the interests of public and highway safety

In order to discharge the above condition, the applicant must enter an agreement with the Council under Section 278 of the Highways Act 1980. The applicant is advised to make early contact with the Highway and Transportation Service by emailing the details to S278Agreements@Leeds.gov.uk. Further information is available on the council's website at <https://www.leeds.gov.uk/parking-roads-and-travel/licences-and-permits>.

38) Prior to the commencement of construction, a Security Plan listing details of what security measures are proposed shall be submitted and approved in writing by the Local Planning Authority. The Security Plan, which shall be prepared in conjunction with advice from the RSES (Register of Security Engineers and Specialists) shall set out measures to control access to the relevant building; cover and protect public areas, this includes monitored CCTV surveillance to protect the structure and fabric of the building

and the public realms outdoors and internally, including parking for the site both during construction and following completion. The measures thereby approved shall be implemented prior to first use of the development and thereafter retained and maintained.

In the interests of security and public safety.



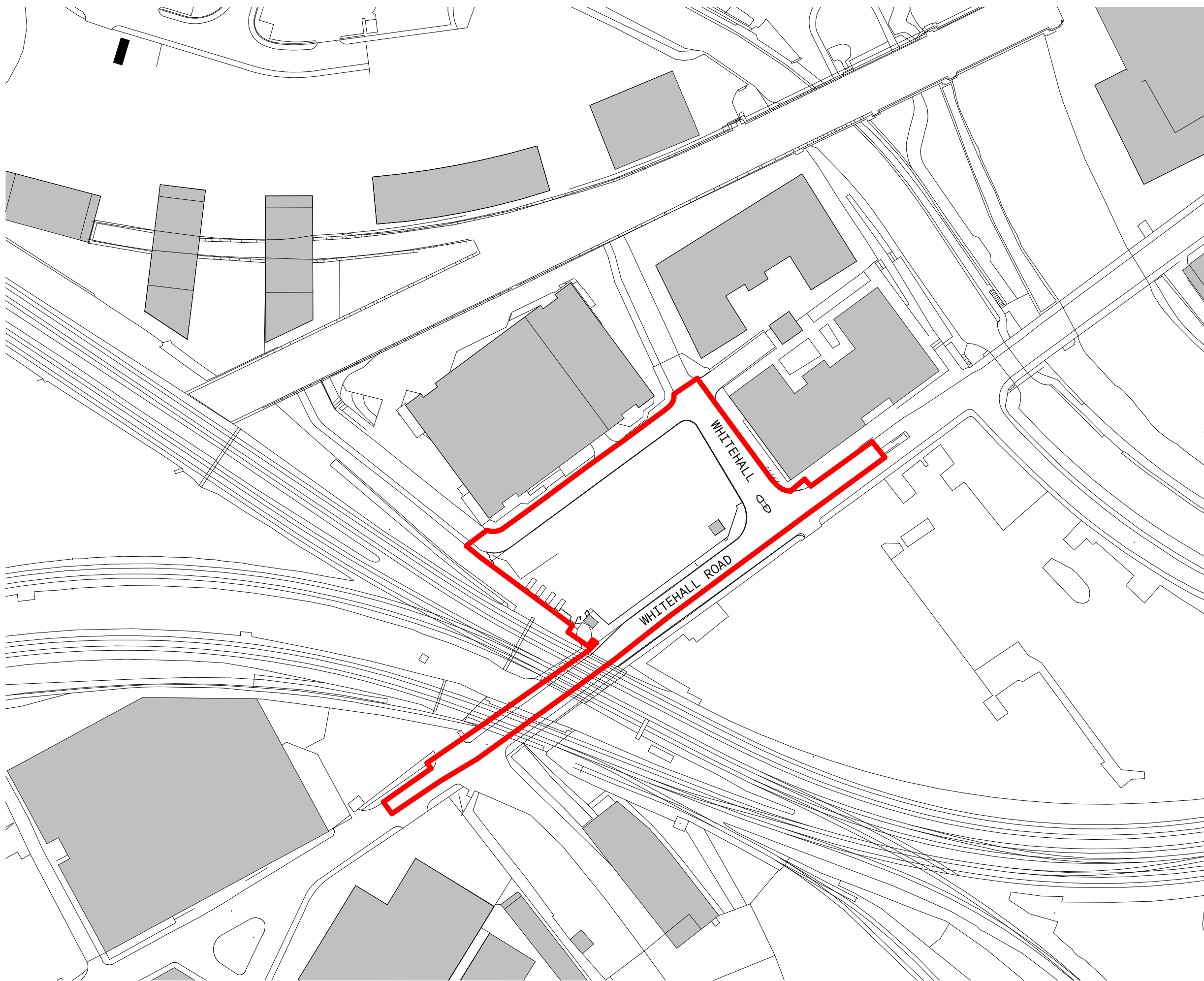
CITY PLANS PANEL

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PRODUCED BY CITY DEVELOPMENT, GIS MAPPING & DATA TEAM, LEEDS CITY COUNCIL

SCALE : 1/2500





The contractor will be held to have examined the site and checked dimensions and levels before commencing construction works.

No assumptions should be made without reference to the architect.

No dimensions should be scaled from this drawing.

Rev:	Description:	Date:	By:
P01	Planning Issue.	04.10.23	KG
P02	Planning boundary line updated following comments received from LCC Highways Officer.	11.12.23	KG



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Status:
PLANNING

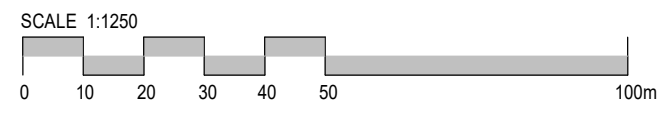
Client:
 BAM Monk Bridge Ltd

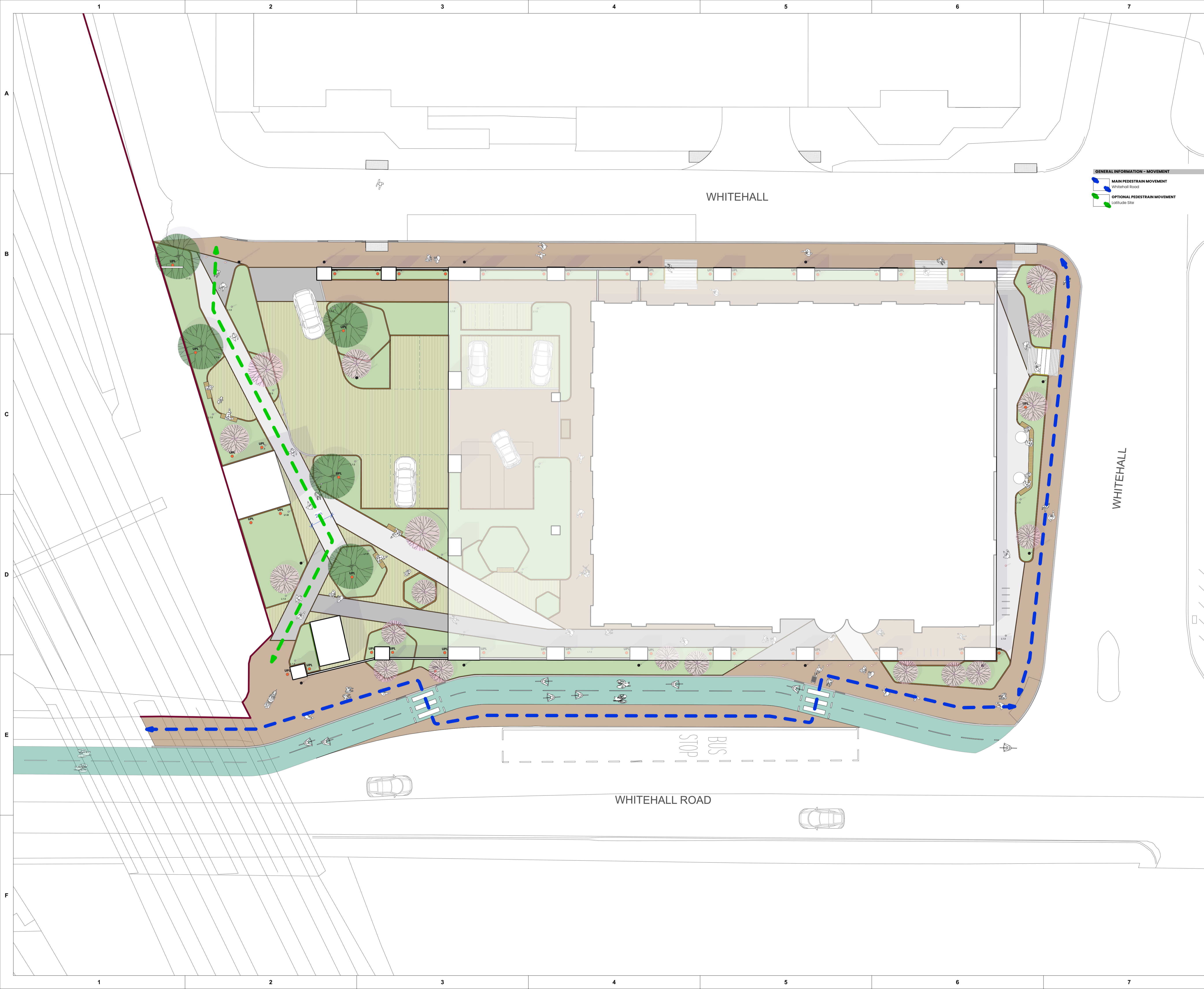
Project Title:
 Latitude Phase Yellow

Drawing Title:
 Site Location Plan

Scale:	Date:
As indicated@A3	29.09.23
Drawn by:	Checked by:
KG	DS

Job No:	Drawing No:	Stb	Rev:
21030G	LYPRS-CCA-ZZ-XX-D-A-05001	S5	P02





- NOTES:**
- DO NOT SCALE FROM THIS DRAWING. ALWAYS WORK TO NOTED DIMENSIONS.
 - ALL SETTING OUT, LEVELS AND DIMENSIONS TO BE CHECKED ON SITE PRIOR TO THE INSTALLATION OF MATERIALS AND ANY DISCREPANCIES ARE TO BE BROUGHT TO THE ATTENTION OF THE CONTRACT ADMINISTRATOR IMMEDIATELY.
 - THIS DRAWING MUST BE READ WITH THE RELEVANT SPECIFICATION DOCUMENTS AND DETAIL DRAWINGS. ANY DISCREPANCIES MUST BE BROUGHT TO THE ATTENTION OF THE CONTRACT ADMINISTRATOR IMMEDIATELY.
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 - ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE STATED.
 - NOTE DRAWING SUITABILITY CODE SHOWN BELOW. DRAWING NOT TO BE USED FOR CONTRACTUAL AGREEMENT WITHOUT A1 STATUS.

THIS DRAWING WAS PLOTTED ON:
20/03/2024

REVISION HISTORY

DATE	REV	ZONE	DESCRIPTION	CHK
28/02/2024	P11		Graphical changes	MC
05/03/2024	P12		Graphical change	MC
20/03/2024	P13		Climber Planting Check	MC



GENERAL INFORMATION

- NETWORK RAIL WALL

HARDWORKS

- HIGH QUALITY PAVING TYPE 1**
SUPPLIER: Tobermore
PRODUCT: Brosear Ground
SIZE: 200x100x65mm
FINISH: Arran Stone
Bond: Stretcher
- DETAIL FEATURE BANDING A**
REF: Exposed Aggregate Insitu Concrete with Creamed Finish
FINISH: Exposed aggregate
DEPTH: 120mm to pedestrian areas
- DETAIL FEATURE BANDING B**
Supplier: Tobermore
Product: Reno
Size: 190 x 50 x 60mm
Colour: Bracken
Bond: Herringbone
- DETAIL FEATURE BANDING C**
Supplier: Tobermore
Product: Tegula
Size: 100 x 100 x 50mm
Colour: Charcoal
Bond: Stretcher
- GREEN GAP PAVING**
Supplier: Hardscape (O&E)
Product: Hydro Limeo 40
Size: 100 x 300 x 100mm
Colour: Standard Grjs
- TACTILE PAVING**
Supplier: Tobermore
Product: Tactile Flags Natural Hazard Warning
Size: 400 x 400 x 60mm
Colour: Natural
Bond: Stretcher
- CYCLEPATH**

EDGING

- CONCRETE KERB** *Level flush*
SUPPLIER: TOBERMORE
REF: FUSION KERB
DIMENSIONS: 145x25x495mm
MATERIAL/FINISH: CONCRETE/TEXTURED
COLOUR: SILVER GREY
- CONCRETE KERB** *Transitional*
SUPPLIER: TOBERMORE
REF: FUSION KERB
DIMENSIONS: 145x25x495mm
MATERIAL/FINISH: CONCRETE/TEXTURED
COLOUR: SILVER GREY

FURNITURE

- PROPRIETARY BENCH**
SUPPLIER: Logis (O&E)
REF: Plateau Bench with arm rests
MATERIAL/FINISH: UK Grown Sustainable Timber
- CYCLE STAND**
SUPPLIER: BROOKAD (O&E)
REF: SHEFFIELD HOOP
MATERIAL: STAINLESS STEEL
FIXING: FOOT FIXED
- CONCRETE STEPS**
SUPPLIER: Tobermore
REF: Granite Steps
Colour: Silver
DIMENSIONS: 1000 x 150 x 400mm
- HANDRAIL**
PRODUCT: 10MM RAILINGS/FENCE PANELS
MATERIAL: FLAT BAR STEEL
ACCESSORIES: SUPPLIED WITH FLAT BAR POSTS
SUPPLIER: LANG-FULTON O&E
- TREE GRILLE**
SUPPLIER: GREEN-TECH (O&E)
PRODUCT REFERENCE: FORTRESS TREE GRILLE
MATERIAL: FINISH: STEEL
- ILLUMINATED BOLLARD**
For full lighting drawing see 7000 series
SUPPLIER: STREETLIFE (O&E)
REF: BRIGHT BOLLARD
DIMENSIONS: 140 x 140 x 900MM ABOVE GL TBC
MATERIAL / FINISH: WEATHERING STEEL TBC
DETAILS TBC BY M&E ENGINEER
- BOLLARD**
SUPPLIER: STREETLIFE (O&E)
REF:
DIMENSIONS: 140 x 140 x 900MM ABOVE GL TBC
MATERIAL / FINISH: WEATHERING STEEL TBC
DETAILS TBC BY M&E ENGINEER

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Client
COOPER CROMAR
 Project
LATITUDE
LEEDS
 Drawing Title

GENERAL ARRANGEMENT

Created By	Checked By	Scale	Sheet Size
MC	ST	1:150	A1

Project Number	Originator	Volume	Level	File Type	Discipline	Number
311	LYR	XX-00-DWG-L-1001				

Work Stage	Status	Revision
Stage 3	SI For Coordination	P13